

SECTION 20D.40 DESIGN STANDARDS

20D.40.10-020 Scope and Authority.

(1) Scope. RCDG 20D.40 contains two sets of design standards: City-wide design standards and Downtown design standards.

- (a) City-wide design standards (RCDG 20D.40.15) apply to developments requiring design review that are located throughout the City and include the Downtown districts.
- (b) The Downtown is divided into several districts. The applicable design standards for Downtown (RCDG 20D.40.100) and the City-wide design standards (RCDG 20D.40.15) apply to developments requiring design review that are located within the Downtown neighborhood.

20D.40.40-020 Signs and Street Graphics.

(1) Intent.

- (a) Signs should be used primarily for the purpose of identification or conveying recognition of a particular development.
- (b) Signs should be consistent with building design and surrounding structures, and be appropriate to the type of activity to which they pertain. Design elements, such as the size, shape, materials, lighting, color, lettering style, and the number and arrangement of signs should present a professional appearance and quality of permanence.

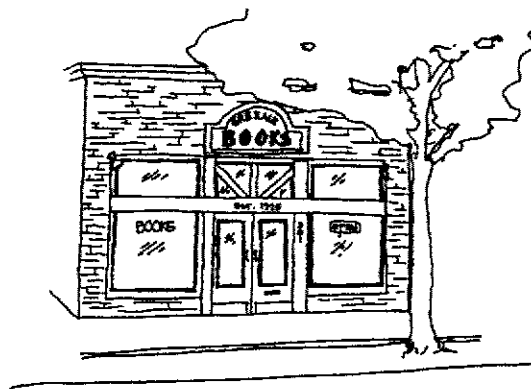


Figure 55: Sign Criteria (1)(b) and (1)(c) of this section.
This is an example of the sign designed to be compatible with the overall building.

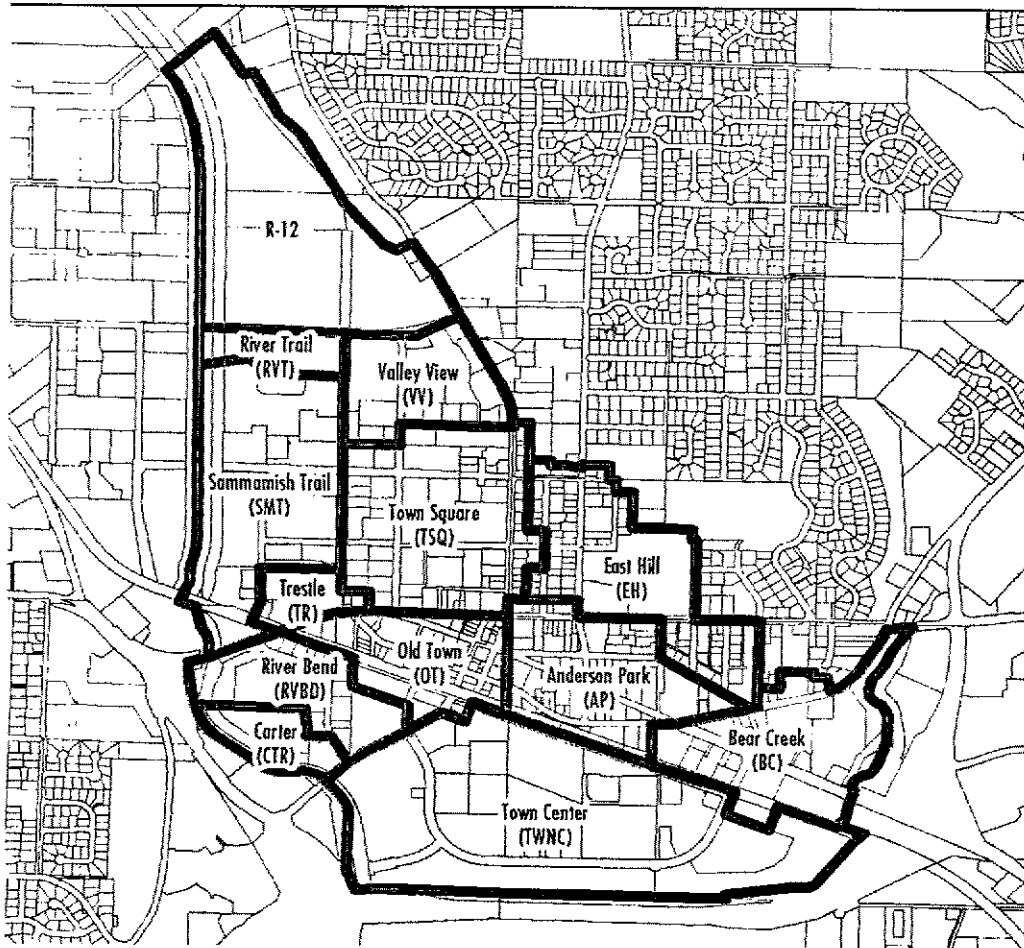
- (c) The shape of a sign should strive for simplicity with all elements constituting an integrated design with the building and landscaping.
- (d) A sign must be legible.
- (e) The scale of a sign should be in proportion with the building or site to which it pertains. Where multiple tenants are involved, signs should be proportionate to the size of the tenant space. For example, an anchor tenant can have the largest sign in a strip mall.
- (f) The number of signs should be minimized in order to avoid visual clutter.
- (g) Sign programs should be adhered to unless:
 - (i) Special circumstances can be substantiated for an individual tenant to deviate from the standard requirements.
 - (ii) Adherence would be out of character with a historic design subarea or a historic landmark.
- (h) A sign should be an understatement in relation to the building and site the sign is identifying. Conversely, a sign should not overshadow its building or surroundings.
- (i) For historic landmarks and the Old Town district, signs should be consistent with the historic character of the landmark and/or district, and should not obscure significant features.

20D.40.100 Downtown Design Standards.

20D.40.100-010 Purpose.

The purpose of this section is to establish design criteria for properties in the Downtown neighborhood that will guide development to be attractive in appearance and functionally integrated, and to promote pedestrian and recreational activities within the neighborhood.

Each district is related to the other through circulation, building scale, and open space relationships. The design criteria for the districts are intended to identify general objectives and specific guidelines that will implement the objectives.



Map of the Downtown Districts.

(Ord. 1993)

20D.40.105 Uses/Buildings Along Burlington Northern Santa Fe (BNSF) Right-of-Way (ROW)

20D.40.105 – 010 Intent

As the BNSF ROW is foreseen to be improved as part of the open space and trail system for Downtown and may also be reactivated for rail use in the future, the intent of this section is to ensure that buildings and uses along the ROW are designed to engage with, and provide, access to the ROW while also ensuring that uses do not hamper rail use in the future if the rail line is reactivated.

In view of the elevated grade and steep side slopes of the portion of the BNSF ROW in the vicinity of the trestle crossing of Redmond Way and the Sammamish River, the provisions of RCDG 20D.40.105 shall not apply to the properties within the portions of the Sammamish Trail and Trestle Districts that adjoin the BNSF ROW.

20D.40.105 – 020 Design Criteria

(1) Building Orientation and Access.

- (a) Buildings along the BNSF ROW should orient to the ROW as well as to the streets, and should include storefronts and entrances to, and through, the building from the ROW/future trail.
- (b) Service areas and mechanical equipment along the BNSF ROW shall be screened with site amenities, architectural elements, and/or landscaping.

(2) Site and Building Design.

- (a) Where a site adjoins the BNSF ROW, the development shall complement the ROW with connecting landscaping, plazas, and other pedestrian features.
- (b) New structures should incorporate structured parking and/or mechanical stacked parking systems inside garages. Parking garages facing the ROW should be attractively designed with ornamental grille work in garage openings or other architectural features on solid walls to ensure that blank, sterile walls do not loom over the ROW.
- (c) Attractive, high quality, building materials and architectural detailing shall be included on all sides of the buildings, including the facades visible from the ROW.
- (d) All ground floor space constructed adjoining the BNSF ROW, including garage floors at the street grade, shall have a minimum ceiling height of at least 10 feet. Ground floor garage space constructed facing and abutting the BNSF ROW that is later converted to commercial/retail space facing/fronting the BNSF ROW/future trail with entry doors and storefronts shall be exempt from providing required parking for the space converted from parking stalls to commercial space, and is exempt from replacing any parking stalls eliminated by the conversion of the ground floor parking stalls to retail/commercial space. (The parking exemption provided in this section shall not be effective until the adoption and implementation of a downtown parking management plan).

20D.40.107 Development Along Downtown Shorelines and Parks.
20D.40.107 – 010 Intent.

- (1) The shorelines and associated parklands of the Sammamish River and Bear Creek are assets to the Downtown neighborhood that should be enhanced, enjoyed, and highlighted.
- (2) Development and uses along these water bodies and next to their associated parklands should add to the attractiveness of the natural features and open spaces.
- (3) In addition to the design standards established for the individual districts, these criteria shall apply to Downtown properties that are in the Shoreline Jurisdiction and Downtown properties that adjoin parklands that are in the Shoreline Jurisdiction.

20D.40.107 – 020 Design Criteria.

- (1) Building Orientation and Access.
 - (a) Development and uses adjoining the Sammamish River, Bear Creek, and their associated parklands shall provide convenient pedestrian access through the site to these features (excepting developments and uses lying between NE 83rd Street (if extended) and the BNSF ROW to the south, which shall provide a pathway or walkway between the development and the Sammamish River trail) unless modified through an approved development agreement where access from the site to the river or creek/parkland is provided.
 - (b) Buildings within 100 feet of a property line of a waterway or park, except single-story retail buildings, shall provide building entrances, balconies, or other such building features or site features (for example: plazas or pedestrian features) on the façade fronting waterways or parks to allow users of the buildings to interrelate with the waterway or park.
 - (c) Buildings next to trails and walkways along waterways and parks shall incorporate pedestrian scaled/friendly architectural features on the facades facing the trails/pathways.
- (2) Site and Building Design.
 - (a) Where a site adjoins a park, the development shall complement the park with connecting features such as landscaping, plazas, and other pedestrian features.
 - (b) Parking facilities next to parks and waterways shall be screened by natural looking landscape buffers with tall trees and understory vegetation between the parking facilities and the parks or shoreline. Planter widths for these screening buffer areas shall be no less than 30 feet wide to accommodate the plant materials in a natural looking planting pattern.

- (c) When surface stormwater facilities are placed near the shorelines and parks, they shall be designed as attractive and naturally occurring ponds and amenities rather than barriers between the use and the shoreline or park.
- (d) Building height is encouraged to be modulated on sites next to shorelines and their associated parks, with lower portions of buildings being adjacent to the trails/parks and taller portions being beyond the Shoreline jurisdiction.

20D.40.110 Valley View, Bear Creek and Trestle Districts.

20D.40.110-010 Intent.

The intent of these districts is to provide for everyday, basic shopping needs and services such as groceries, pharmacies, and other convenience retail goods and services. Development and redevelopment in these districts should be compatible with the goal of meeting the shopping and service needs of the community and surroundings, as well as with the long term vision of encouraging a more pedestrian-supportive, mixed-use environment in these districts. As the three districts are located at the three entry points to the Downtown neighborhood, development and redevelopment of buildings and centers at these gateways should enhance the visual appeal of these entrances to the Downtown.

20D.40.110-020 Design Criteria.

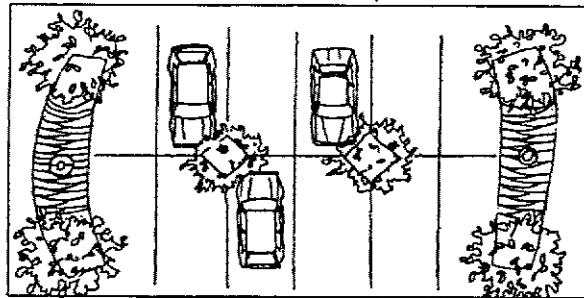
(1) Building Orientation and Access.

- (a) Buildings are not required to be sited on the pedestrian system, but shall provide safe and accessible pedestrian connections to it.
- (b) Service areas and rear entrances shall not be oriented to public areas.
- (c) Orient buildings toward the streets and locate parking on the side or rear whenever possible.

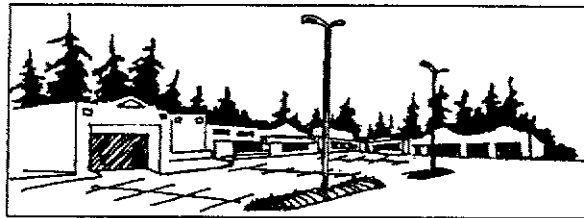
(2) Building and Site Design.

- (a) Develop sign programs to coordinate and consolidate signs in order to increase visibility from automobiles and minimize visual clutter.
- (b) Minimize elements in signs such as colors that are too brilliant or other features that would distract from the entry feature.
- (c) Landscaping on streets should be simplified to allow adequate visibility from automobiles to businesses.
- (d) Parking lots should be landscaped to screen parking from street views.

- (e) Building and site design of properties located at the entrances to Downtown shall also orient facades, open space, and pedestrian features towards the streets in order to enhance the gateway nature of the sites.
- (f) One tree for every four parking stalls and landscape planting areas shall be provided to provide visual relief from large asphalt areas, shade pavement, and reduce impervious surface area.



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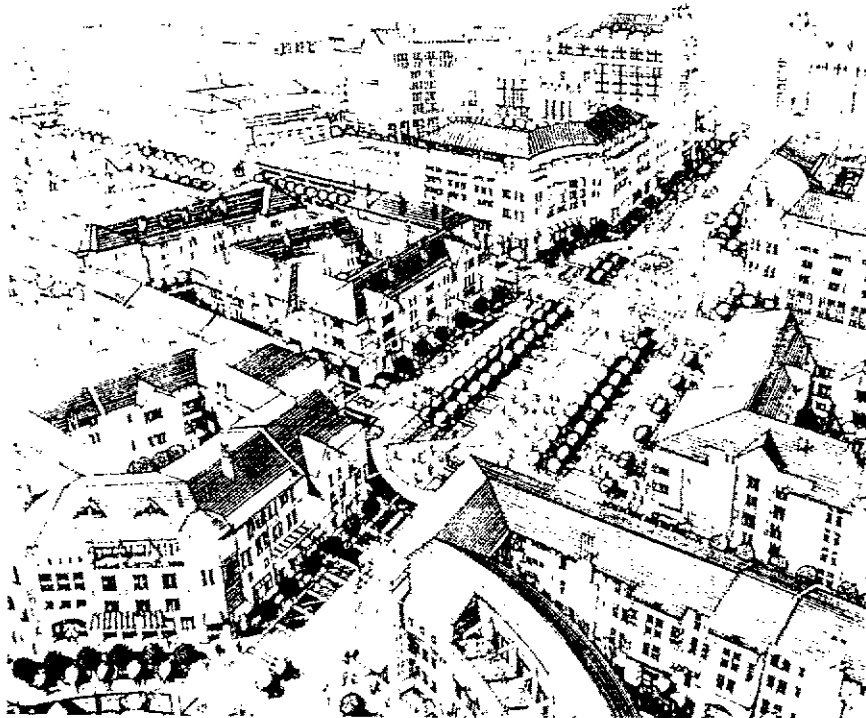
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The existing strip mall parking lot retrofit with new landscape installation of one tree for every four parking spaces.

- (g) Pedestrian access from parking areas should be safe and clearly defined.
- (h) Landscaped medians should be provided where access and traffic movements allow.

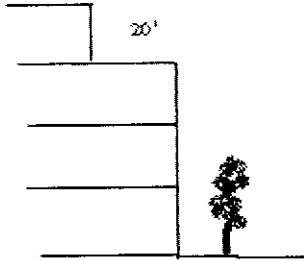
- (i) Conflict between pedestrians and automobiles should be avoided by minimizing vehicle crossings of pedestrian access ways. (Ord. 1993)
- (3) Mixed-Use Villages. Development proposals in these districts may be allowed increased building height, to a maximum of 4 stories, provided the proposal meets the following criteria:
 - (a) The site must be a minimum of 2 acres in area.
 - (b) The site plan and conceptual building elevations demonstrate there will be creation of a traditional downtown pattern by providing new internal private or public streets with curbside parking, street trees, and buildings fronting on street sidewalks and plazas. Within these developments, new internal streets and pathways shall be provided approximately every 300 feet to establish/enhance a finer urban grid between existing Downtown streets bounding the sites. See conceptual example below.



CONCEPT OF MIXED-USE VILLAGE

- (c) On sites that include existing grocery stores, pharmacies, and other convenience retail uses meeting daily needs, spaces for such uses shall be provided in the new development proposal. Retention of grocery stores is of the highest priority.

- (d) Buildings taller than 3 stories fronting on existing public streets shall include building height modulation to reduce building scale at the street edge. Upper floors, higher than 3 stories, shall be setback 20 feet from the main building face.
- (e) This section (paragraph 3) shall not be effective until the adoption and implementation of a downtown parking management plan).

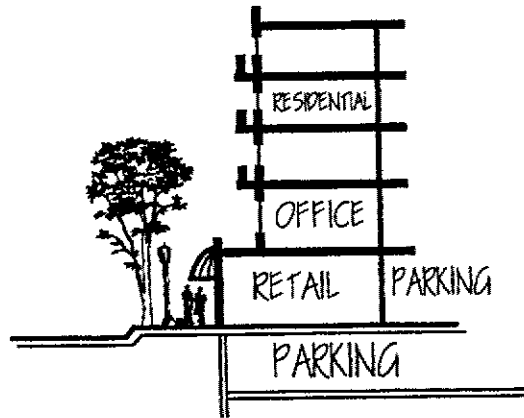


- (f) In areas where a street roadway may be more than one (1) story above the ground floor elevation of a building because of topography, such as the southwest corner of NE 90th Street and Woodinville-Redmond Road, building height may be increased to a maximum of 5 stories on the lower side of the site, provided the height of the building does not exceed 4 stories along the higher street elevation and provided the net developable floor area of the property covered by the conceptual master plan does not exceed the floor area allowed by the maximum building height of 4 stories.

20D.40.115 Sammamish Trail, Anderson Park, and Town Square Districts.

20D.40.115-010 Intent.

- (1) These districts are intended to provide areas of greater density adjacent to the retail core of Old Town and to foster the evolution of mixed-use residential/office neighborhoods in an attractive, high quality, pedestrian friendly-environment within walking distance of daily retail and recreational opportunities, and transit services.
- (2) Density and building height are increased in the Town Square District nearest the center of Downtown to encourage development of structures of larger scale and size. Structured parking is encouraged as heights increase over four stories to reduce the impact of on-site parking and allow greater open space at the ground level. Retail uses are also encouraged at ground floor levels to further encourage pedestrian activity.

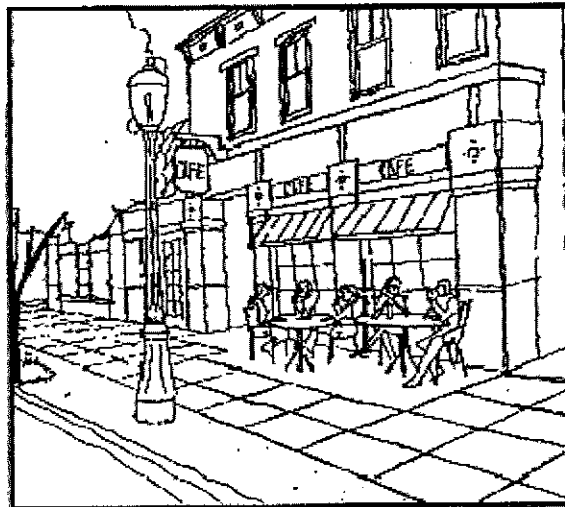


- (3) Heights and density are moderated in peripheral areas of the Town Square district to complement and transition with the adjoining residential and convenience shopping areas. (Ord. 1993)

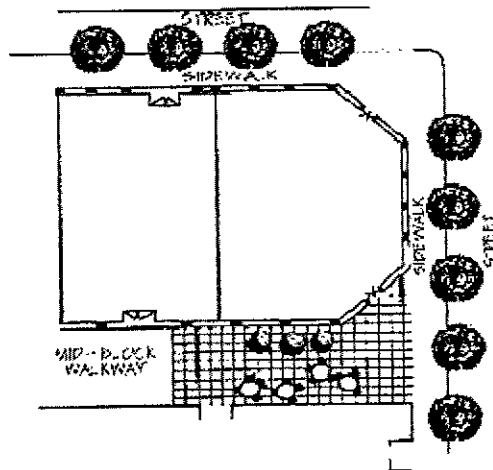
20D.40.115-020 Design Criteria.

(1) Building Orientation and Access.

- (a) Buildings should abut the pedestrian system on at least one side. Buildings fronting on Type V pedestrian streets per RCDG 20C.40.105, Downtown Pedestrian System, shall abut the street front sidewalk and orient the primary entrance, or entrances, toward the street.



- (b) Vehicle access should be designed to minimize interaction of vehicles with pedestrians particularly in relation to the pedestrian system or should be screened with pedestrian amenities.
- (c) Parking structures should not front on the internal block connections of the pedestrian system or should be screened with pedestrian amenities such as artwork, trellises, awnings, benches, and the like.
- (d) Driveway openings to arterials should be minimized.
- (e) When ground floor commercial spaces abut mid-block pedestrian connections, entrances to the commercial spaces should be oriented toward the mid-block connections as well as toward the streets. The area alongside the required mid-block sidewalk should be developed as a usable plaza with a combination of hardscapes, planters, and seating areas.



- (f) Buildings with ground floor residential uses should be set back an additional 6 feet from the minimum required sidewalk along the street, per the Pedestrian System Map 20C.40.105, to ensure that there is adequate separation from the ground floor unit and pedestrian way. The finished floor of the ground floor residential units should be elevated at least 2 feet above sidewalk grade to provide additional privacy for the residences at the street level. See example below.



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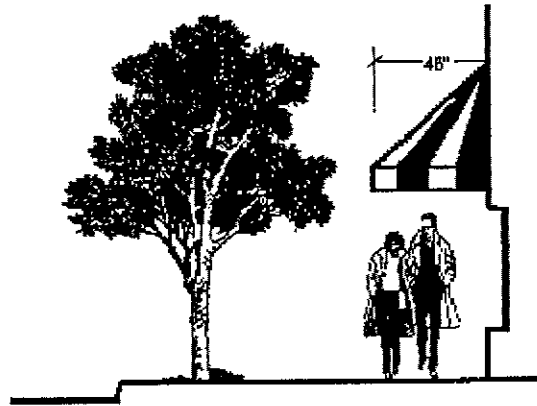


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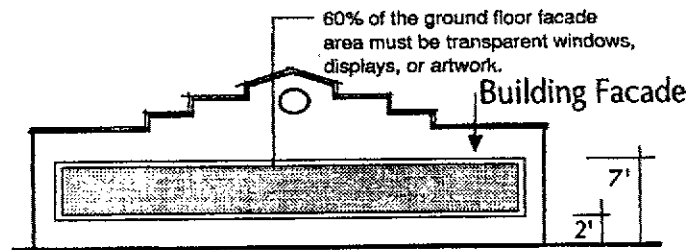
(2) Building and Site Design.

- (a) Buildings should be sited in a manner compatible with adjacent buildings and the streetscape. Consideration should be given to size and height relationships with adjacent buildings.
- (b) Open space and landscaping should be coordinated and linked wherever possible, particularly in relation to public areas and the pedestrian system.
- (c) Landscaping should be designed with consideration of shade and sun; plazas and outdoor areas should be oriented to sunlight.
- (d) Outdoor and ground floor areas should be designed to encourage outdoor activities such as vendors, art displays, seating areas, abutting retail activities, and other features of interest to pedestrians.
- (e) Taller buildings (above 5 stories) should have adequate separation to maintain a sense of openness, adequate light, and views.
- (f) Building materials shall be visually appealing, high quality, and durable. A high proportion of exterior building materials should be materials such as brick, stone, and masonry. Building materials and colors should minimize light reflection and glare.

- (g) Facades should be articulated into increments through the use of architectural features such as bay windows, offsets, recesses, and other devices which break or minimize scale.
- (h) Rooftops should incorporate features that soften rectilinear forms and effectively screen mechanical equipment from view.
- (i) Buildings should be designed to provide for weather and wind protection at the ground level. Buildings fronting on Type V pedestrian system segments should provide pedestrian weather protection by way of awnings and overhangs, a minimum of 48 inches in depth. The elements should be complementary to the building's design and design of contiguous weather protection elements on adjoining buildings. Materials and design should engender qualities of permanence and appeal.



- (j) Provide a variety of shapes, angles, and reliefs in the upper stories of structures over four stories.
- (k) For non-residential ground floor uses, windows, rather than blank walls, shall be provided on the street level to encourage a visual and economic link between the business and passing pedestrians. A minimum of 60% of the length of the storefront area facing streets (between 2 feet and 7 feet above the sidewalk) shall be in non-reflective, transparent glazing.



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- (l) Column and bay spacing along street fronts should be provided no greater than 30 feet apart in order to maintain a pedestrian-oriented scale and rhythm.
- (m) Trails – Bicycle. Facilities for parking and locking bicycles should be provided and be readily accessible from bicycle trails.

(Ord. 1993)

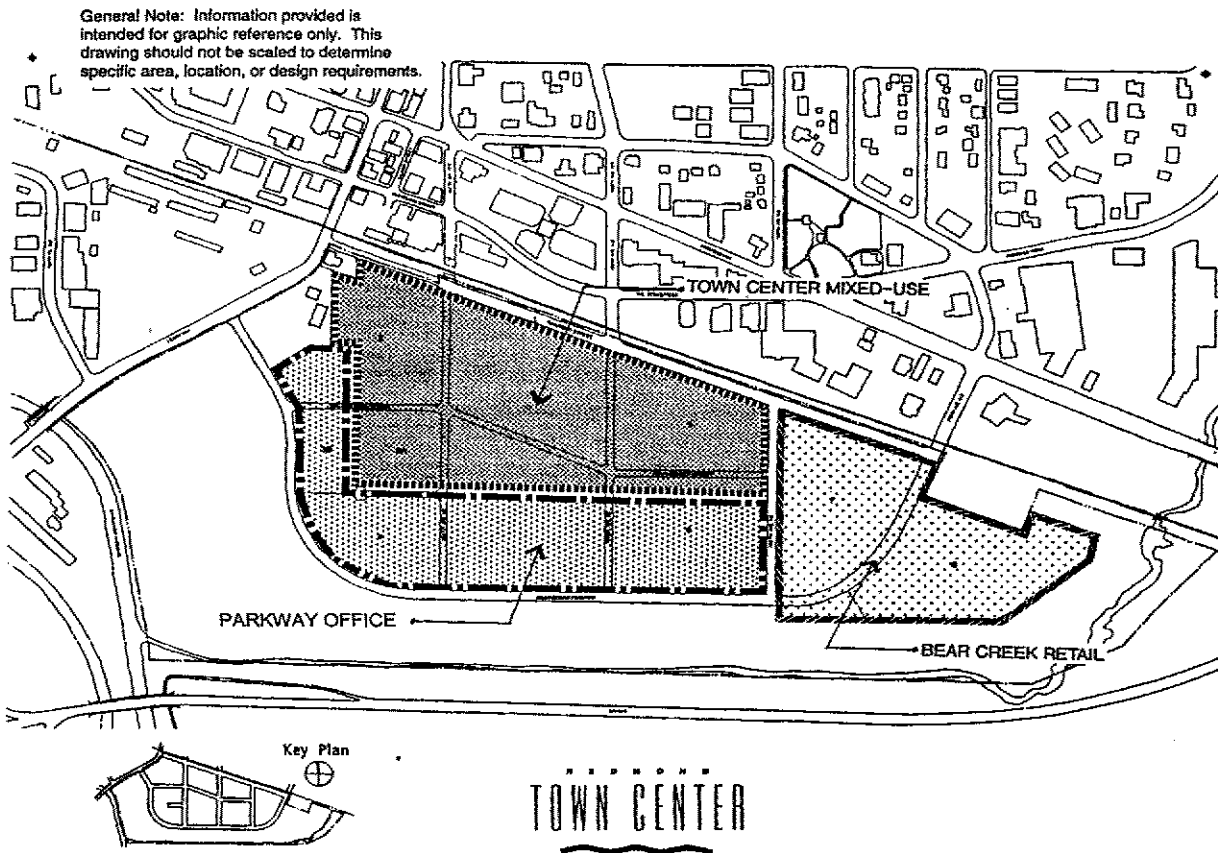
20D.40.120 RESERVE

Ordinance No. 2302, Exhibit B

20D.40.125 Town Center District.

20D.40.125-010 Intent.

(1) The Town Center District consists of three sub-areas as shown and described below:



- (a) The Town Center Mixed-Use area design concept stresses a pedestrian oriented, open-air complex that mirrors the existing Old Town transportation network and the architectural character and scale of the historic portion of the Downtown neighborhood. Primary design features for the Town Center Mixed-Use area include storefronts along roadways, curbside parking, pedestrian plazas, and sidewalk designs that integrate into building architecture. (See Figures 1, 6, and 7.)

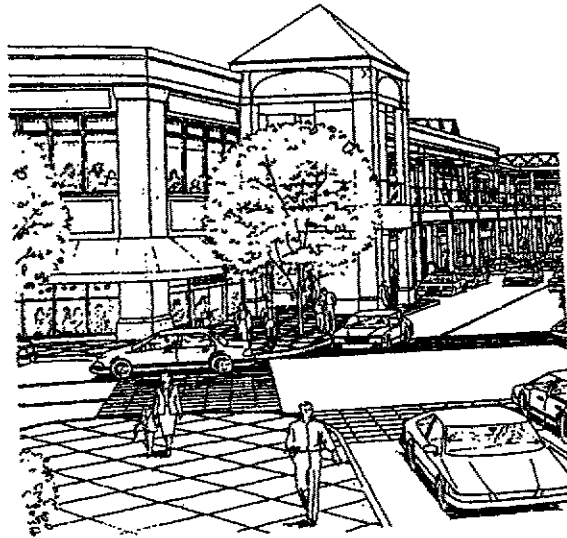


Figure 1

- (b) The Parkway Office area design concept features multi-level office buildings and an urban gateway facing SR 520. Building height, location, and architectural character are intended to create a strong urban perimeter and a varied urban texture connecting the site with the Downtown. (See Figure 2.)

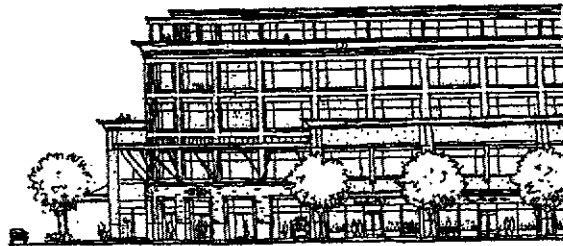


Figure 2

- (c) Bear Creek Retail area provides for auto-oriented retail tenants. The freestanding buildings with surface parking are distinct from the other two areas. However, architectural character, featured design elements, and pedestrian linkages incorporate a design commonality with the rest of the site. (See Figure 3.)



Figure 3

- (2) Gateway to Downtown. The Parkway Office area also functions as a gateway to the City from SR 520. Development in this area should complement the other components of this gateway, Marymoor Park, and Bear Creek, by providing attractive, interesting urban activity. Development should be consistent with the natural environment by minimizing glare, providing indirect lighting, avoiding intense signage, and providing a soft edge where the urban and natural environments meet. (See Figure 4.)

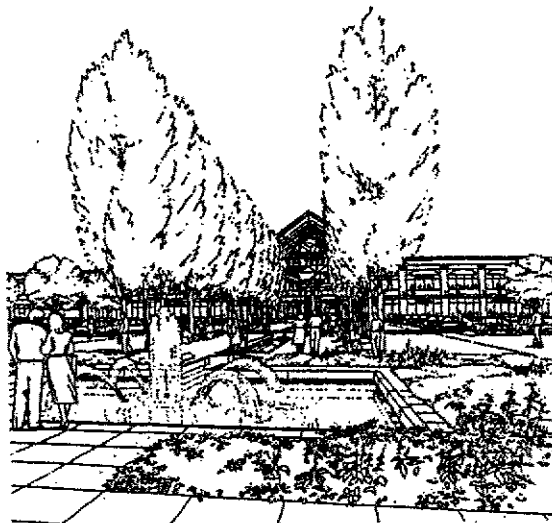


Figure 4

- (3) Downtown Integration. Connection to existing roads including landscape treatment, road surface, sidewalk size and placement should respect and extend the existing grid system, streetscape, and character consistent with current standards and regulations. Development in the design area should further City goals for the following sub-areas:
- (a) Leary Way. Leary Way between the Sammamish River and the BNSF right-of-way should remain as a “green gateway” to the City of Redmond.

- (b) Northern Boundary – Leary Way to 164th. Building siting should maintain continuity of building frontage in order to integrate new development with the Old Town district.
- (c) Northern Boundary – Leary Way to 170th Avenue.
 - (i) This area should provide linkage capability between existing public roadways north of BNSF right-of-way and private roadways south of same. These new alignments should provide extension of the established visual corridors.
 - (ii) New connections on the site to existing north/south roads in this area should be compatible with the character of the existing older improvements.
 - (iii) Retail buildings located at the northern edge of the site within the Town Center Mixed-Use area will establish functional and visual continuity with the downtown. The character of the new buildings will be compatible with older existing buildings. (See Figures 5 and 8.)

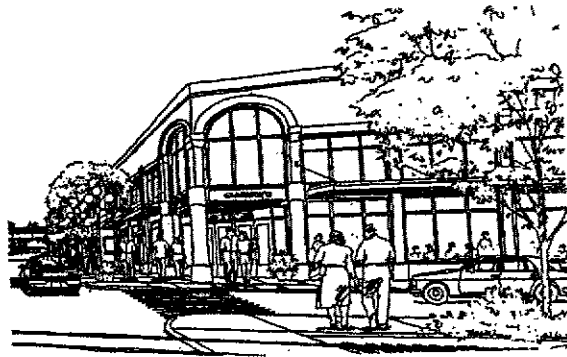


Figure 5

- (d) Bear Creek.
 - (i) The edge along Bear Creek should be kept as a natural area, with uses limited to passive activity and trail/pathway connections.
 - (ii) Signage in this area should be limited to traffic, safety, and directional information, or be consistent with the public recreational use of the area.
 - (iii) Structures consistent with and supporting passive use of this area may be allowed, but should be kept to a minimum.
- (e) Sammamish River.

- (i) The edge along the Sammamish River should serve as an extension of existing activity on the Sammamish River Trail just north of this design area. Uses should include trail and pathway activities.
 - (ii) Signage should be limited to traffic, safety, and directional information or be consistent with the public recreational use of the area.
 - (iii) Structures consistent with and supporting trail/pathway activities may be allowed, but should be kept to a minimum.
- (f) BNSF ROW – Pedestrian Crossings. Design and construct City-approved architectural/urban design features, walkways, and landscaping on 164th Avenue NE and other locations as determined to be necessary. (Ord. 1993)

20D.40.125-020 Design Criteria.

(1) Architectural Guidelines.

- (a) **Siting of Buildings.** Buildings should be sited to enclose either a common space or provide enclosure to the street. Consideration should be given to the relationship between buildings and adjacent open space areas. All designs should appear as an integrated part of an overall site plan. (See Figures 6, and 7.)
- (b) **Building Form.** The roofline of buildings should be modulated to avoid large areas of flat roof and should include interesting architectural features. Consideration should be given to the appearance from nearby hillsides. (See Figures 1, 3, 6, and 10.)

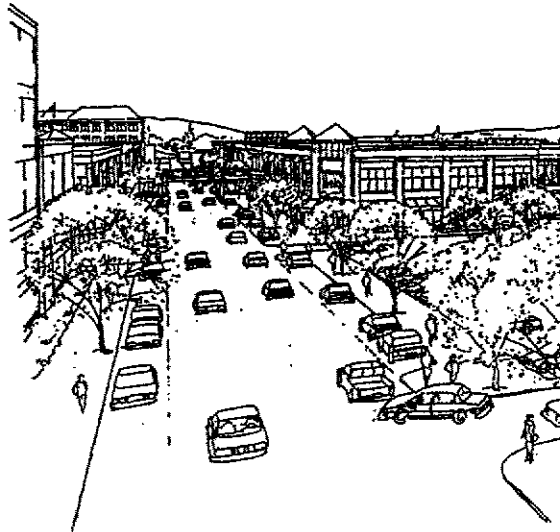


Figure 6



- (i) Encourage varieties of shapes, angles, and reliefs in the upper stories of structures over four stories.
- (ii) Large buildings should avoid continuous, flat facades.
- (iii) Avoid the use of false fronts and large blank walls.
- (iv) Facades should be articulated into increments through the use of architectural features such as bay windows, offsets, recesses, and other devices which break or minimize scale.
- (v) The ground floor of buildings should provide pedestrian interest and activity. The use of arcades, colonnades, or awnings to provide pedestrian protection is encouraged. Column and bay spacing along street fronts should be provided no greater than 36 feet apart in order to maintain a pedestrian-oriented scale and rhythm. (See Figures 1, 5, 7, and 9.)



Figure 7

- (vi) Building design should complement the character of adjoining Old Town.
- (vii) Building material, colors, scale, and form should be harmonious with the surrounding environment in Old Town.
- (viii) Buildings and facades in the Town Center Mixed-Use area should be a combination of brick, stucco-like finishes, smooth finished concrete, and architectural metals. Building facades in the Town Center Mixed-Use and Parkway Office areas should have a greater proportion of voids (windows) than solids (blank walls) on pedestrian levels. Buildings and facades in the Bear Creek Retail area should be primarily masonry products with concrete and architectural metals used for detailing if desired. In all design, there should be emphasis upon the quality of detail and special form in window treatments, columns, eaves, cornices, lighting, signing, etc.
- (ix) Buildings and the spaces between them should relate easily and openly to the external public areas or plazas.
- (x) The scale of all structures in relationship to other structures and spaces is important. The scale should be two to three stories in the retail core. Some variation in heights contributes to the variety and complexity of the environmental experience, and is encouraged. (See Figures 1 and 6.)
- (xi) The development of ground level view points on each building level which take advantage of solar access and views of the site's open spaces is encouraged.
- (xii) The integrity of a building as an individual structure or as part of a series of buildings should be respected.
- (xiii) Buildings as a whole should be seen as significant architectural elements.
- (xiv) Storefront design and materials should be allowed to be unique while maintaining the character of the building facade of which they are a part.

- (c) **Building Entry.** Orient building entrances to the street in a manner which provides easily identifiable and accessible pedestrian entryways. Highlight building entrances through landscape or architectural design features. Building entries should be designed in conjunction with the landscape treatment of pedestrian ways in the parking areas that directly relate to the entry.
- (d) **Public Art.** Encourage public art in public areas of the Town Center district, particularly in and around the Town Center Mixed-Use area.
- (e) **Building Orientation.** Uses in the Town Center district should be oriented externally as well as internally (as is applicable) by using outward facing building facades, malls, entrances, and other design techniques.
- (i) **Buildings in the Town Center Mixed-Use and Parkway Office areas** should abut the sidewalks on at least one side and orient the primary entrance, or entrances, toward the street.



Figure 8

- (f) **Building Colors and Materials.** Building colors and materials shall be selected to integrate with each other, other buildings in the Old Town district, and other adjacent commercial areas, while allowing a richness of architectural expression for the various buildings.
 - (i) Buildings should be constructed of materials that minimize light reflection and glare.
 - (ii) Care should be taken to avoid clashing colors on individual buildings and between adjacent buildings.
- (g) **Windows and Displays.** Windows and display areas shall be located along pedestrian routes to enhance the pedestrian experience. (See Figures 5, 7, and 9.)

- (i) Storefronts should be visually open wherever practical. Stores should use enough glass so that the activity inside the store is obvious to the passerby. In all cases, merchandise should be easily visible to pedestrians.
- (ii) Windows shall be provided on the street level in the Town Center Mixed-Use buildings rather than blank walls to encourage a visual and economic link between the business and passing pedestrians. A minimum of 60 percent of ground floor facades facing streets in the Town Center Mixed-Use area shall be in nonreflective, transparent glazing. Where windows cannot be provided, art work in window boxes may be used with site plan review approval. (See Figures 5, 7 and 9.)



Figure 9

- (h) Future Development Pads. Future development pads shall be designed to relate to the rest of the project's architecture and will provide pedestrian-scale exterior features.
- (i) Design Consistency. Each phase of the development shall be designed to be consistent with, but not necessarily the same as, the balance of the project architecture, including materials, colors, and general style.

- (j) Pedestrian Features. Provide pedestrian-scale external features, including such items as window and glass display cases, street furniture, and covered walkways, and avoid large blank walls.
 - (k) Outdoor Pedestrian Areas. The outdoor pedestrian areas shall include special paving treatments, landscaping, and seating areas. (See Figures 1 and 4.)
 - (i) Outdoor and ground floor areas should be designed to encourage outdoor activities such as vendors, art displays, seating areas, outdoor cafes, abutting retail activities, and other features of interest to pedestrians.
 - (l) Site Entrances. Entrances to Town Center development shall be emphasized with landscape treatments to strongly indicate the pedestrian orientation of these areas.
 - (i) Architectural/urban design treatment of 166th shall encourage pedestrian circulation from the project to the Cleveland Street Retail area.
 - (m) Rooftop Mechanical Equipment. Rooftop mechanical equipment will be screened in a manner which enhances the overall architectural design, and rooftops will be of a color that reduces glare and other types of visual impact on the adjacent residentially developed hillsides.
 - (i) Rooftops should incorporate features which soften rectilinear forms and effectively screen mechanical equipment from view.
- (2) Transportation Guidelines.
- (a) Vehicular.
 - (i) Street Configurations.
 - (A) Streets that are above existing grade should be designed in a manner to reduce visual impact of pavement area such as using landscaping or berms.
 - (B) Encourage alignment of all streets to minimize the removal of all existing significant, healthy trees.
 - (C) Streets should not be wider than four travel lanes with the appropriate number of lanes at intersections between the district and areas targeted for integration with the downtown.
 - (D) Vehicular circulation shall relate the various uses on the site to each other. Streets shall be designed to enhance viability of the project components.
 - (E) Vehicle access to parking areas should be designed to minimize interaction of vehicles with pedestrians.

(ii) Parking – Surface.

- (A) Where possible, locate parking behind buildings and away from areas of high public visibility. Landscape and screen surface parking areas visible to the public.
- (B) Parking areas should include landscape areas and be designed to minimize long, straight, monotonous rows. The size and location of parking areas should be minimized and related to the group of buildings served.
- (C) Parking areas should include a clear circulation network to guide pedestrians to the building entrances.
- (D) Visual impact of surface parking areas should be minimized from the SR 520 corridor.
- (E) Landscaping shall be provided to screen surface parking areas and provide transition between the project and surrounding areas particularly when viewed from SR 520, Leary Way and adjacent hillsides.

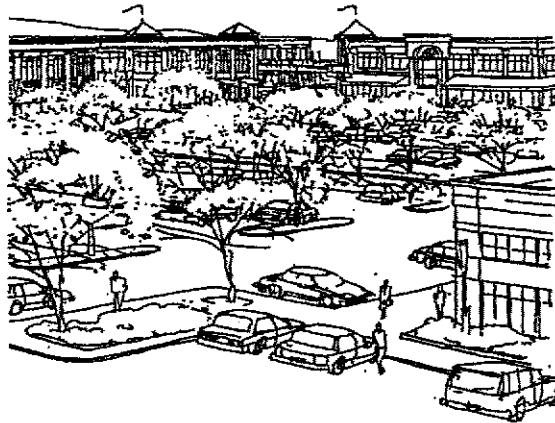


Figure 10

- (F) Parking lots should be landscaped to screen parking and provide visual relief from large asphalt surfaces.
- (G) Pedestrian access from parking areas should be safe and clearly defined.
- (H) Landscaped medians should be provided where access and traffic allow.

- (I) Conflict between pedestrians and automobiles should be minimized by designing streets to provide well-defined pedestrian walkways and crosswalks that reduce vehicle speeds.
 - (J) Open space and landscaping should be coordinated and linked wherever possible, particularly in relation to public areas and the pedestrian system.
 - (K) To minimize visual impacts where surface parking is utilized, project design should incorporate screening and landscape treatment of surface parking areas.
 - (L) Design and locate parking areas in a manner that will break up large areas of parking and encourage shared parking with existing downtown uses.
 - (M) Patrons of the retail center shall be allowed to use parking while patronizing other businesses in the Downtown. No rules, signage, or penalties shall be enacted by Town Center to preclude this parking allowance.
- (iii) Parking – Structured.
- (A) Structured parking should be designed to avoid undifferentiated planes. The scale of parking structures should be modulated by interruptions of the facades, setbacks, and lowering the first level below the existing grade (where the water table allows) to reduce total height.
 - (B) Facades of parking structures should include a landscape treatment in addition to architectural screening from the SR 520 corridor.
 - (C) Parking structures shall have landscaping around the perimeter which will correspond to adjacent land uses and activities. Landscaping shall include, but not be limited to, a combination of shade trees, evergreen trees, shrubs, groundcovers, deciduous native and ornamental shrubs, and vines to further screen the structures.
 - (D) The top floor of parking structures should include landscape screening in areas such as along the cornice and on the deck, either by trees or a screening trellis treatment.
 - (E) Provide walkways in parking floors which have curbs or other barriers to protect from vehicular intrusion.
 - (F) For security, pedestrian routes should be visible and avoid enclosed, hidden areas. Emergency call boxes should be available.
 - (G) At least 50 percent of the parking provided for the entire site should occur in parking structures. The ratio of minimum structured parking shall be maintained

for all phases of development of the Town Center Mixed-Use and the Parkway Office areas.

- (H) Parking structures should be enclosed with retail or office uses on the exterior. Or where this enclosure is not feasible, the visual impact should be softened with landscaping or screening.

(b) Pedestrian.

(i) Linkages.

- (A) Link proposed development to walkways, trails, and bicycle systems in the surrounding area by connecting and lining up directly to existing linkages, closing gaps and treating crossings of barriers (such as the railroad, Bear Creek Parkway, and driveways) with special design treatment, minimizing barriers, designing with consistent materials, widths and locations, and providing safe, easy, and clearly identifiable access to and along the linkages. Safe, convenient, and attractive connections to Marymoor Park, the Sammamish River Trail, and the Bear Creek Trail system should be provided.
- (B) The sidewalk system shall be emphasized with landscape treatments to provide readily perceived pedestrian pathways through and around the Town Center district.

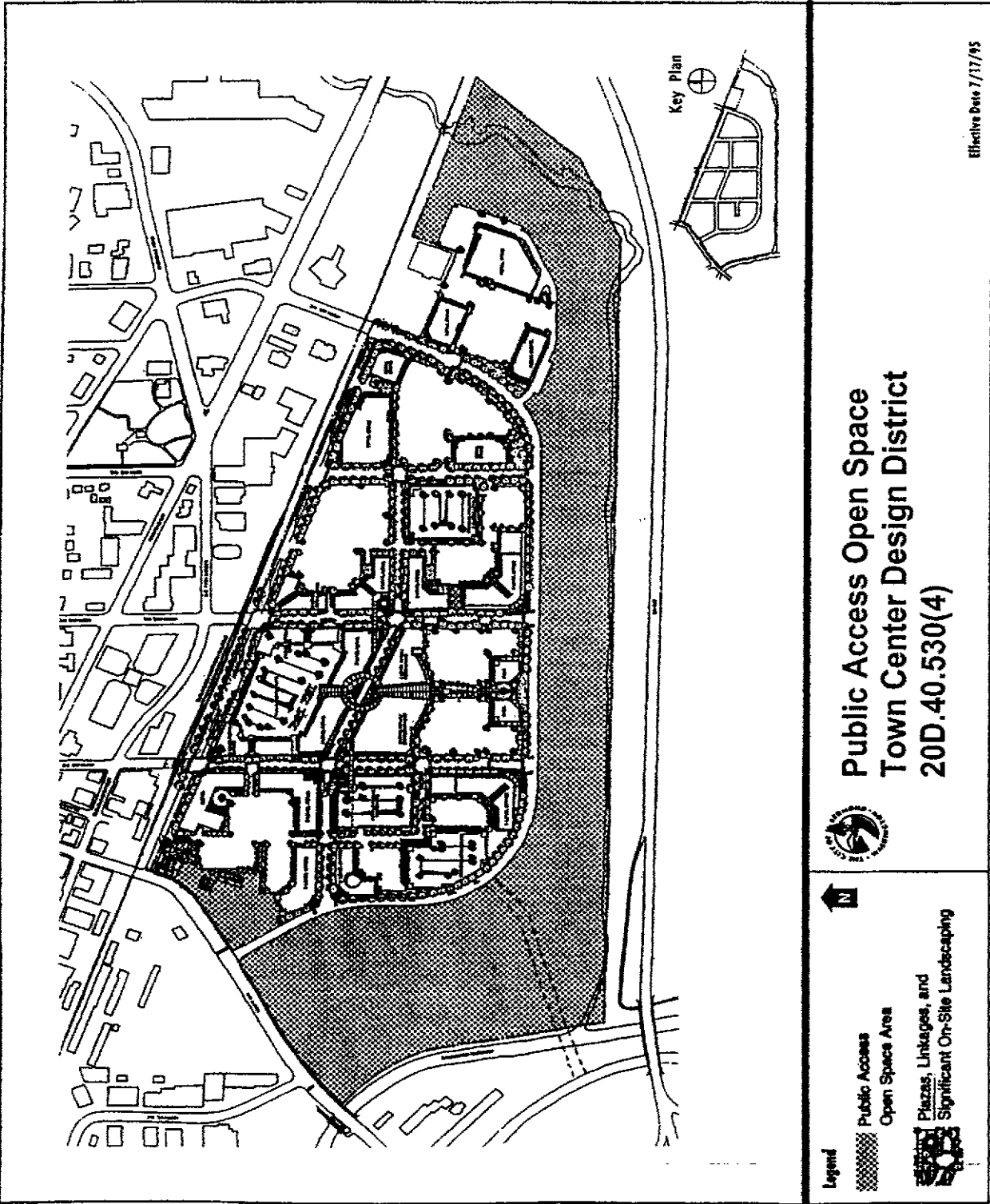
(ii) Sidewalks.

- (A) When extending an existing sidewalk, the new walkway should meet current standards and regulations where there is sufficient right-of-way, and should be constructed of a material and dimension which is compatible with, and improves upon, the existing character.
- (B) Sidewalks should meet similar standards to those of the approved pedestrian linkage system.
- (C) Paving of sidewalks and pedestrian crossings should be safe and constructed of a uniform material that is compatible with the character of the district. The private use of sidewalk rights-of-way areas may be appropriate for seasonal cafe seating or special displays.
- (D) Encourage alignment of new sidewalks to minimize the removal of all existing significant, healthy trees.

(iii) Arcades, Colonnades, Canopies.

- (A) In areas of high pedestrian activity, devices for protection from weather are encouraged. Consistent treatment within a single area is also encouraged in order to provide a strong identity of space. (See Figures 1, 5, 7 and 9.)
 - (B) Buildings should be designed to provide for weather and wind protection at the ground level. Buildings fronting sidewalks should provide pedestrian weather protection by way of arcades, colonnades, or canopies a minimum of 48 inches in depth. The elements should be complementary to the building's design and design of contiguous weather protection elements on adjoining buildings. Materials and design should engender qualities of permanence and appeal.
 - (C) Awnings or sunshades should be harmonious to the design of the building to which attached, and should be in keeping with the character of the building. Materials should be durable, long lasting, and require low maintenance. Back-lit awnings are discouraged.
- (iv) Enclosed Malls. The design of enclosed malls should allow pedestrians through access during hours of retail center operation while still maintaining security.
 - (v) Trails – Pedestrian. Special design treatment and appropriate safety features should be afforded the pedestrian trail crossings at public rights-of-way and at the BNSF right-of-way tracks.
 - (vi) Trails – Bicycle. Facilities for parking and locking bicycles should be provided and be readily accessible from bicycle trails.
 - (vii) Trails – Equestrian. Width of the trail should be adequate for two riders side-by-side in order to avoid earth compaction and vegetation deterioration. Equestrian trails should be separate from pedestrian and bicycle trails.
 - (viii) Plazas/Pedestrian Malls. Plazas, pedestrian malls, and other amenity open spaces shall be developed to promote outdoor activity and encourage pedestrian circulation between the Town Center district and the balance of the Downtown.
- (3) Landscape Guidelines.
- (a) Urban Landscape Treatment. Building entries, primary vehicular entries, and building perimeters should be enhanced with landscaping which could include ornamental vines, groundcovers, shrubs and/or trees selected for their screening, canopy, spatial enclosure, and seasonal variation.
 - (b) Site Furnishings. Benches, kiosks, signs, bollards, waste receptacles, street vending carts, water fountains, lighting standards, perch walls, sidewalks, pathways, trails and special water features should be designed to be compatible elements of like materials and design.

- (c) Perimeter Landscaping. Landscaping on the perimeter of the site will create a transition between the project and the surrounding area.
 - (d) Landscaping on Streets. Landscaping on streets should be simplified to allow adequate visibility from automobiles to businesses.
 - (e) Trees, Plants and Flowers. The use of potted plants and flowers as well as street trees are encouraged, but should not impede pedestrian traffic.
- (4) Open Space Guidelines.
- (a) Tree Retention and Open Space Landscaping. Preserve existing natural features, particularly healthy mature trees and stream courses.
 - (i) Preserve a minimum of 100 percent of all trees within the public access open space as identified in the Public Access Open Space Area Plan. This area includes the cluster of trees along the east side of Leary Way for the purpose of preserving the corridor's green gateway image, and the healthy trees along the Bear Creek and Sammamish River corridors. Trees that cannot be retained due to approved street and/or utility construction shall be replaced with native nursery stock of similar or like variety at a one to one ratio, with tree sizes in accordance with RCDG 20D.80.20-080, pursuant to a landscape plan approved in conjunction with site plan review. Trees removed as a result of construction activities which are intended to be preserved shall be replaced per RCDG 20D.80.20-080. Replacement trees shall be located in the immediate vicinity as is practical.



- (ii) Minimize new grading in this area.
 - (iii) Install landscape screening between this open space area and adjacent parking areas.
 - (iv) Encourage passive recreation including a walking trail, bicycle trail, seating and rest areas, pedestrian lighting, and site furnishings. Connections should be provided to the Justice White House, Town Center Mixed-Use area, Marymoor Park, Sammamish River Trail system, and other open space areas.
 - (v) The “soft edge” landscape treatment to the south of Town Center along Bear Creek shall provide for a true transition between the natural, riparian area of the creek to the more urban Mixed Use Retail Area.
 - (vi) The informal nature of the west, south, and east portion of the site should be maintained by retaining native materials and random planting of compatible plant materials consistent with the Downtown Neighborhood.
- (b) Justice White House/Saturday Market. The areas around the Saturday Market and Justice White House shall be retained as open space. Areas at the Justice White House should encourage active and passive recreation. These areas should connect to other open spaces, trails, and the Mixed Use Retail area.
- (c) Sammamish River. Open space should be retained along the Sammamish River and enhanced by:
- (i) Providing grade separation for trails at all appropriate and feasible locations;
 - (ii) Making connections to other open space zones;
 - (iii) An ongoing stormwater outflow monitoring program for private drainage systems. The monitoring program shall consider specific contaminants which may likely be present in the runoff and shall be revised periodically as appropriate.
- (d) Bear Creek. Open space along Bear Creek should be retained and enhanced by:
- (i) Encouraging passive recreation areas and activities and discouraging active recreation.
 - (ii) All stormwater swales and recharge areas should be integrated with the natural environment.
 - (iii) Protecting vegetation of the riparian habitat in this zone by limiting access to the creek to designated access points.
 - (iv) Providing connections to Marymoor Park, the Sammamish River, other open spaces, and Town Center.

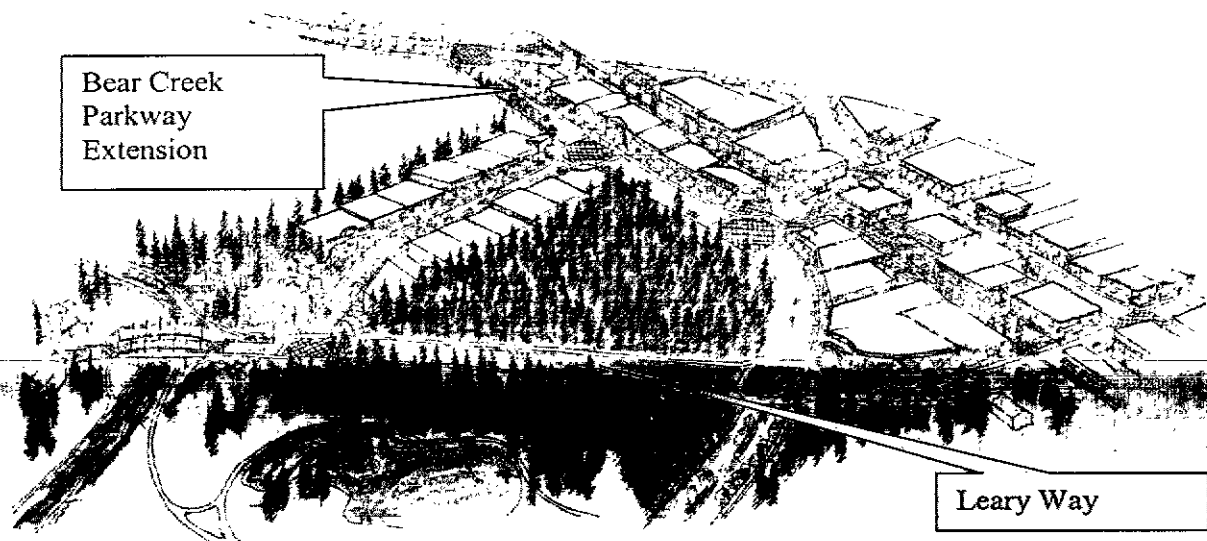
- (v) Facilities within this area should include a pedestrian pathway, bicycle path, equestrian trail when required, passive water access area, seating, and site furnishings.
 - (vi) An ongoing stormwater outflow monitoring program for private drainage systems. The monitoring program shall consider specific contaminants which may likely be present in the runoff and shall be revised periodically as appropriate.
 - (e) Public Access Open Space. Public access open space should be retained, enhanced, and made available for public use in this district as shown in the Public Access Open Space Area Plan.
 - (i) At least 44 acres shall be preserved by easement to the City or controlled by other methods that would permanently assure the open space to the City. This downtown public access open space shall serve as a visual amenity and passive recreation open space.
 - (f) Open Space Acreage. Public access open space as shown in the Public Access Open Space Area Plan shall include a minimum of 44 acres. This will include natural areas inclusive of the floodway, and the areas around the Justice White House and the Saturday Market.
- (5) Signage Guidelines.
- (a) Sign Kiosks. Sign kiosks, which provide direction to groups of tenants, are encouraged as a means for discouraging numerous individual directional tenant signs and providing clear directions. These kiosks shall be located in high traffic areas and be consistent with the design theme of the development.
 - (b) Symbols and Colors. International symbols and colors should be used whenever applicable.
 - (c) SR 520 Signage. Signage for the subject proposal shall comply with the SR 520 Corridor Signage Policy adopted by the City Council.
 - (d) Signs will be evaluated according to the overall impact, size, shape, texture, lettering style, method of attachment, color, and lighting in relation to use, the building and street where the sign will be located, and the relationship of the sign to other signs and other buildings in the vicinity. Adherence to or enhancement of sight lines both parallel and perpendicular to the sidewalks will be considered. The primary reference will be to the average pedestrian's eye-level view, although views into or down the street from adjacent buildings should be an integral feature of any review.
 - (e) Signs should be simple, clear, and direct. Extruded aluminum or plastic signs are not appropriate. Careful use of neon may be acceptable.

- (f) Signs should not hide or obscure the architectural elements of the building.
 - (g) Signs attached or hung from the arcades, colonnades, and canopies perpendicular to the sidewalk should be encouraged.
 - (h) Projecting, double-faced or three-dimensional signs may be allowed if they are integrated into the overall streetscape and do not destroy sight lines. Symbolic three-dimensional signs such as a shaving mug, barber pole, pawn shop symbol, or other symbols illustrating the product being sold on the premises are acceptable provided they meet other sign guidelines. When possible, signs should reflect the character and the use within the structure.
 - (i) Sign lighting should be energy efficient.
- (6) Lighting Plan.
- (a) A lighting plan and program which encourages nighttime pedestrian movement between the adjacent commercial areas, particularly Leary Way and 166th Avenue NE, shall be maintained.
 - (b) Parking lot and security lighting shall be screened by buildings and other means to minimize light intrusion onto the Sammamish River and Bear Creek.
 - (c) Street lighting should relate in scale to the pedestrian characters of the area. The design of the light standards and luminaries should enhance the design theme. (Ord. 1993)

20D.40.130 River Bend District.

20D.40.130-010 Intent.

- (1) Leary Way, between the Sammamish River and the BNSF right-of-way, should remain as a "green gateway" to the City of Redmond. Setbacks from Leary Way should be adequate to preserve significant stands of trees on the west side of the road to preserve the "green gateway" image.
- (2) Signage on Leary Way for traffic control, directional information, and business identification should be kept at a minimum, in both size and number. (Ord. 1993)
- (3) Bear Creek Parkway, between Redmond Way and Leary Way should also be treated as an urban gateway to the Downtown neighborhood, evolving with a more traditional ground floor storefront character as it redevelops over time.
- (4) Ground floor building facades along the westerly Bear Creek Parkway extension, as it is envisioned to extend from Redmond Way on the west to Leary Way on the east, should be pedestrian friendly and pedestrian scaled, with narrow storefronts along the sidewalk edges, awnings for pedestrian comfort, and ample storefront windows.



- (5) Developments on larger interior lots, with relatively little existing street frontage, should be designed as mixed-use villages with internal pedestrian friendly streets and pathways serving mixed-use buildings.

20D.40.130-020 Design Criteria.

(1) Building Orientation and Access.

- (a) Vehicle access and parking should be designed to minimize curb cuts along Leary Way and to minimize the loss of existing trees within the district.
- (b) Where significant trees exist between the street and building sites, driveways from the primary streets to on-site parking areas should be designed in a tree-lined parkway manner, complementing, preserving, and taking advantage of remaining stands of trees.
- (c) Building orientation should complement any remaining stands of trees.

(2) Building and Site Design.

- (a) The setback from the property line on this portion of Leary Way shall be at least 100 feet where there is an existing grove of significant trees (six trees within a quarter-acre area). No parking shall be allowed within the setback. No clearing, grading permit, or site plan approval shall be granted until the approval authority determines that the existing trees will be preserved to the maximum extent possible within the setback.

- (b) Site layout for individual parcels within the district should be designed to provide reciprocal vehicular and pedestrian access to and from adjoining lots in order to achieve a

unified circulation plan which minimizes curb cuts on Leary Way and Bear Creek Parkway, and provides pedestrian connections between uses in the district.

- (c) New landscaping should be designed to complement and reinforce any remaining groves of trees within the district. Large trees and shrubs, both evergreen and deciduous, should be used to complement the natural character of this district.
- (d) Buildings and site layout should be designed to complement the mixed-use retail character of the adjoining Old Town District, which includes features such as narrow ground floor storefronts on pedestrian-oriented streets, shops fronting on street sidewalks, and pedestrian-scale architecture. On larger lots with relatively little existing street frontage, building and site layout should be designed to extend the traditional downtown street grid into the site, with storefronts fronting on internal streets and paths.
- (e) Column and bay spacing along street fronts should be provided approximately 30 feet apart in order to maintain a pedestrian-oriented scale and rhythm of storefronts.
- (f) Pedestrian connections from buildings within this district should be provided to the adjoining Old Town and Town Center Design districts. (Ord. 1993)

20D.40.135 RESERVE

20D.40.140 RESERVE

20D.40.145 North Point, Carter and East Hill Districts

20D.40.145-010 Intent

- (1) These residential districts are intended to provide a variety of housing choices. These districts are intended to provide comfortable and attractive living environments with a variety of housing types, and only limited mixed-use opportunities, in developments that include more typical residential features such as front yards, landscaping, and ground related patios and porches.
- (2) The intent for the East Hill district is to allow Downtown's original single-family neighborhood to evolve from its existing collection of houses into a denser multi-family neighborhood that honors the character of the original single-family houses and neighborhood character. New multi-family development, as well as conversions of the existing single-family residences to commercial uses, should maintain and enhance the residential look and character of the district.
- (3) In the East Hill District, ensure that conversions of single-family residences to commercial uses are done in a manner that does not negatively affect the "residential" character of the existing structures and front yards.

20D.40.145-020 Design Criteria.

(1) Building Orientation and Access.

- (a) Buildings should be oriented toward the streets.
- (b) Primary entrance of buildings shall be oriented to the street; and ground floor units that have patios along the street should be designed with walkways from the patios to the street sidewalk in order to establish a “front door” relationship with the streets. See example below.



- (c) Yards of ground floor residential units fronting on streets should be designed to provide a separation between public and private spaces, while also providing maximum territorial view of the street front from the units' private open spaces.
- (d) Vehicular access to sites from streets should be minimized. Alley access should be utilized where possible and feasible to minimize curb cuts along street fronts and to maximize the amount of available curbside parking.

(2) Building and Site Design.

- (a) Buildings shall provide focus and articulation of primary entrances along the street front. Primary entrances shall not be hidden from the street or accessed via the side yard.

- (b) Buildings and private usable open spaces shall be appropriately setback from property lines to provide ample light, air, and privacy for the on-site tenants and adjacent neighbors.
- (c) Residential buildings should be designed to provide maximum amounts of usable open space on site, or nearby, for the residents and to provide front yards complementary to the size and scale of the developments.
- (d) Parking shall be enclosed within subterranean or semi-subterranean parking whenever possible to provide maximum amounts of usable open space on parking decks and to minimize building heights.
- (e) Building facades and roof lines shall be articulated to provide visual interest and relief as well as to promote long-term neighborhood and community pride.
- (f) Mixed use buildings should be designed with a residential character, in terms of building materials, roof design, fenestration, and front yard landscaping.

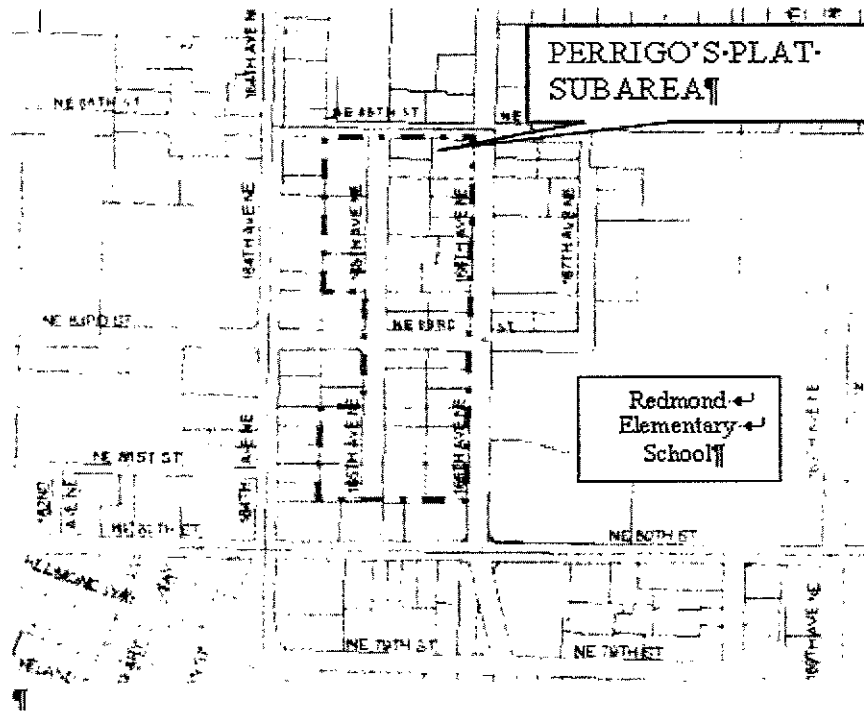


- (g) Open exterior corridors fronting and visible from streets and public spaces shall be minimized in use and length in order to reduce their negative visual impacts on surrounding streets and properties. They should be placed on the interior of developments whenever feasible.
- (h) On-site parking, both within structures and outdoors, shall be screened from public streets. Surface parking spaces shall not be located between the building and the street.
- (i) Common usable open spaces and yards shall be adequately lighted and visible from private spaces within the developments to ensure security and safety of the tenants, residents, and their guests. (Ord. 1993)
- (j) Conversions from single-family residences to commercial uses shall not significantly alter the residential character of the original single-family structures or front yards. Additions

and/or modifications to these structures for commercial use shall maintain or enhance the residential look of the property.

20D.40.145-030 Perrigo's Plat Subarea.

1. Intent. The intent of this section is to retain the quaint and inviting character of this subarea, which includes a variety of building styles and features, generous front yards, sense of quiet and peacefulness, large trees along the street, and unique bungalows, by establishing design criteria that encourage new development to emulate and enhance the traditional residential character of the subarea.
2. Design Criteria. In the Perrigo's Plat subarea of the East Hill district, as shown below, these additional standards shall apply.



- (a) New developments, redevelopments, and remodeling projects shall emphasize features typically found in neighborhoods with older bungalow, craftsman and Victorian styled homes, including but not limited to pitched roofs, dormer windows, front doors with porches or stoops oriented to the street, chimneys and double-hung or multi-paned windows.

- (b) New development, redevelopment, and remodeling projects shall use high proportions of visually appealing, high quality, exterior building materials, such as brick, stone, and masonry, on at least the base of the building. The cladding materials of the body shall also be durable, attractive, and complementary to the early 1900's craftsman/Victorian residential style including brick, horizontal siding, and wood (appearing) shingles.
- (c) For developments consisting of multiple aggregated lots, the width of buildings fronting the street should generally be no wider than 100 feet to ensure that building mass remains consistent with the existing neighborhood character and scale. When there are buildings at the rear of the lot, behind those fronting the street, the buildings in the rear may be modulated in stories to provide a sense of building separation.
- (d) New development, redevelopment, and remodeling shall be designed with front yards characteristic of single-family neighborhoods, including street trees along the street, with trees and landscaping in the front yard.

20D.40.150 Old Town District.

20D.40.50-010 Intent.

The intent of this section is to preserve the existing Historic Landmarks in the district, ensure that development and redevelopment complement the historic character and function of Downtown's original pedestrian retail area, and to maintain or enhance the historic integrity of the Leary Way corridor. These standards are established to maintain and enhance the traditional "main street" character of the neighborhood, which includes continuous pedestrian-oriented storefronts and pedestrian-scaled streetscapes along the streets.

20D.40.150-020 RESERVE

20D.40.150-030 Site Design

- (1) Intent. The core of this district developed between the late 1800's to the mid 1900's encompasses a variety of periods. The intent of site design standards within the Old Town district is to maintain the historic patterns of land development and general character of the district that was created during this timeframe.
- (2) Design Criteria.
 - (a) Building setbacks. Setbacks shall reflect historic patterns. Buildings shall front on the public sidewalk. Structures should be built to the side lot line or the side setback should be 5 feet or less not including access width. Rear setbacks may allow for a narrow alley access, for service entrances or for several stalls of parking. Additions or other remodels are encouraged to match this pattern when feasible.
 - (b) New structures should incorporate structured parking or mechanical stacked parking systems. Visible structured parking shall not front along the sidewalk edge except to allow placement of parking entrances. Structured parking should preferably be underground or in backs of structures. Surfaces masking parking shall continue design

rhythms, colors, materials, and trims used in the other parts of the structure. Surface lots should be located to the rear of structures.

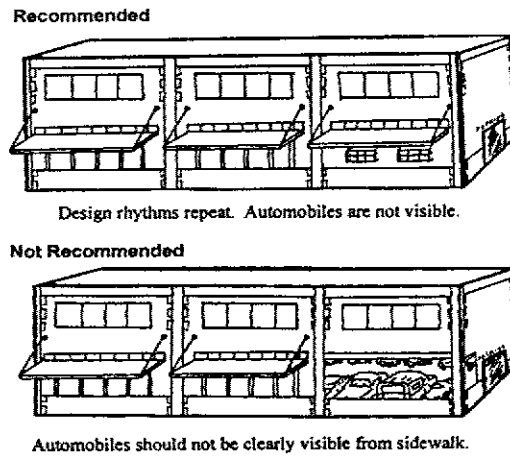


Figure 1. Parking Design

20D.40.150-40 Building Design

- (1) The intent of building design standards within the Old Town district is to ensure that Historic Landmark buildings maintain their prominence within the district, and to ensure the character of the original downtown core is maintained. These building design standards shall apply to all new structures. These standards will also apply to additions or other remodeling whenever it is feasible and in context with the existing structure.
- (2) Building orientation.
 - (a) Entrances shall front towards the sidewalk.
 - (b) Peaked roofs, primary roof ridgelines, or roof ridgelines predominant to the viewer shall run from front to back.

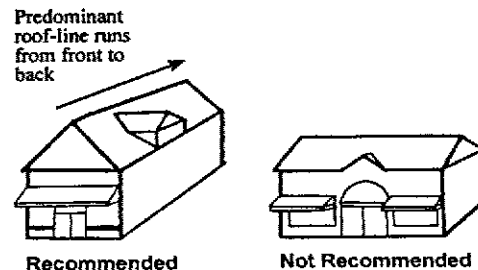


Figure 2. Roof Ridgelines for Peaked Roofs

- (3) Building scale. New construction should be in scale with existing historic patterns. In cases where the scale of the building is larger, techniques should be used to break up the scale of the building to complement existing patterns. These techniques can include variations in roof height, vertical columns to break up facades, changes in roof or parapet detail, use of smaller repeating window patterns, use of fascia on the facade, façade articulation, and stepping back or modulating of upper stories.

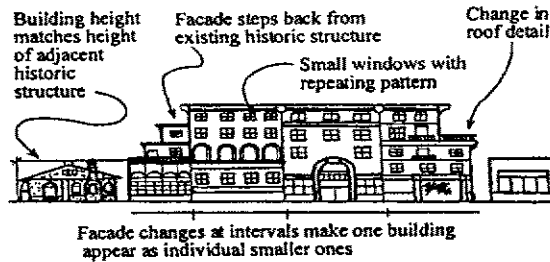


Figure 3. Building Scale

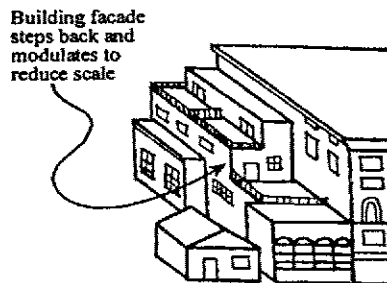
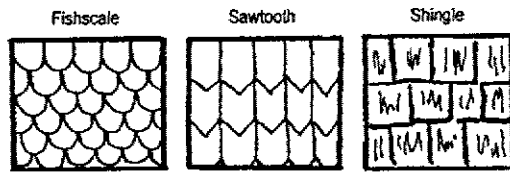


Figure 4. Methods to reduce scale

- (4) Building design, details, and materials.
- (a) Buildings should incorporate vernacular architectural styles from the periods reflected in the district.
 - (b) Buildings should incorporate materials similar to those used on existing historic structures. This includes brick, stucco that is used in the art deco style, wood, and stone. Use of modern materials is acceptable to the extent that it differentiates the new structures from historic ones without substantially changing the visual character reflected in the district.

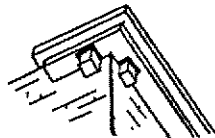


Wood sidings common to the historic period of Redmond. These sidings were often used individually or in combination with clapboard or board and batten. These sidings or reasonable facsimiles in modern materials are encouraged.

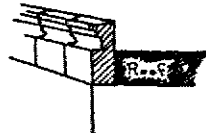
Figure 5. Illustration of Common Wood Siding materials

- (c) For 1 to 1-1/2 story structures, a false front is allowed on peaked roofs.
- (d) Hipped roofs are discouraged unless they are in context with the period of architecture reflected in the district.
- (e) Additions and other remodeling to existing buildings should closely match or complement the historic architectural styles reflected in the district.
- (f) Buildings shall incorporate details prevalent in the architecture reflected in the district.

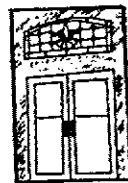
Cornice Details such as this are encouraged. Use of molding or brackets also enhance historic character.



Parapets and parapet details such as this are encouraged. The detail of the parapet is enhanced when it differs in material or color.



Use of columns, or pilasters can enhance door detail. Use of such detail as pediments or architraves for doors is also encouraged.



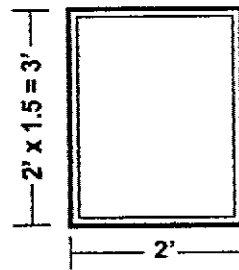
Transoms above doors and use of decorative glass are details that can enhance historic character.

Figure 6. Details encouraged to enhance Historic character

- (g) Details such as parapets and bands, windowsills, and doorframes shall consist of contrasting materials or color to enhance detail.

- (h) Size of individual windowpanes or false panes shall not exceed the smaller sizes typical of early period glass separated by mullions and rails. For windows above the first story, the height or vertical dimension shall be at least 1 to 1-1/2 times the width or horizontal dimension, or mullions or stiles should separate the panes to create the sense that the dimension of the vertical element exceeds the horizontal one.

Example of Ratio Requirements:



Window Height must equal or exceed Width by 1 1/2 times.

Figure 7. Window Dimensions above First Story

- (i) Building fenestration shall consist of numerous individual windows or larger windows having multi-panes to avoid large areas of solid surface.
- (j) For brick, stone, and art-deco stucco structures, upper story windows shall be punched or recessed except for bay windows.
- (k) Details such as soldier course or solid lintels and sills shall be used around windows in brick and stone structures.

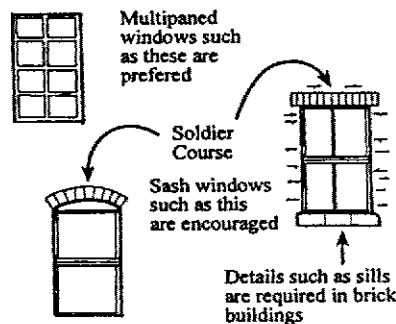
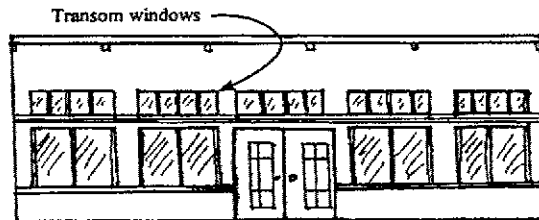


Figure 8. Multipaned Windows and Sill and Lintel treatments

- (l) Windows shall not start at floor level and shall repeat historic patterns that allowed views to the interior of a storefront.
- (m) Transom windows are encouraged.



Window shall not start at floor level. This illustration shows a typical starting point for many historic buildings.

Figure 9. Transom Windows and Windows Above Floor level

- (n) Preferred window dimensions and type are illustrated in Figure 10.

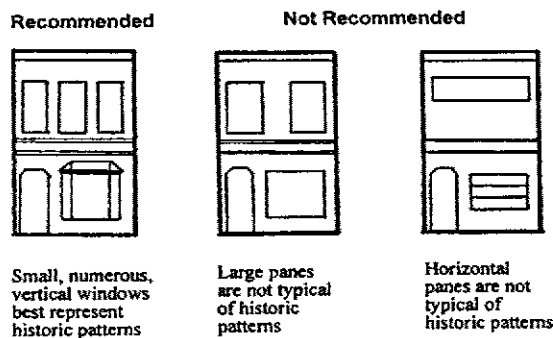


Figure 10. Preferred Window Dimensions and Types

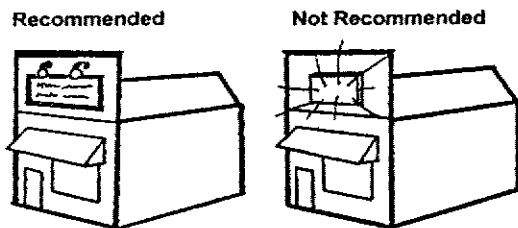
- (o) Recessed entrances are encouraged for storefronts. Porches, courtyard entrances, or stoops are encouraged for ground floor residential units where allowed.
- (5) Building Color.
- (a) New buildings, additions, and other remodels may contrast with that of existing buildings to the extent that use of color still allows a Historic Landmark building to remain prominent within the district.
 - (b) Preferred colors are those that reflect the historic patterns of the district. However, other colors, particularly on non-historic structures, will be allowed if they are complementary to surrounding structures.

20D.40.150-50 Signage and Awnings

(1) Intent. To ensure that all buildings in the Old Town district have signage compatible with, or complementary to, the Historic Landmark, the period of the structure, or with the general period reflected in the district.

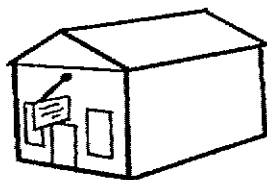
(2) Design Criteria.

- (a) Size of signs shall comply with RCDG 20D.160 Signs unless either compliance would be detrimental to the integrity of a Historic Landmark or when a design concept is more in keeping with the architecture reflected in the district.
- (b) Preferred signage. Window signage, painted signage on wooden facades, wood signs, signs on awnings, signs lit by sources other than the sign itself, or decorative signs hung perpendicular to the building façade, are encouraged. Wood facsimile products may be an acceptable substitute. Use of backlit plastic wall signs, extruded aluminum, changing message, or other newer technology signs should not be allowed. Use of neon signs should be limited to window signs or art deco styled buildings.



Use of painted signs lit by separate source such as illustrated here are encouraged. Highly modern signs such as this backlit plastic sign are strongly discouraged.

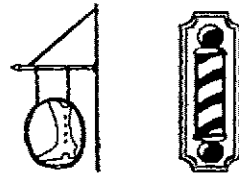
Figure 11. Preferred Signage



Signs hung perpendicular to front facade are encouraged.

Figure 12. Perpendicular Signage

- (c) Use of signs with logos illustrating trades is encouraged.



Trade logo signs are encouraged

Figure 13. Typical Trade Logo Signs

- (d) Awning design, color, and lettering should be compatible with or complementary to the awning styles, colors, and lettering that are typical of the periods reflected in the district.
- (e) For awnings, preferred styles are wedge awnings, bullnose entrance awnings, and flat decorative metal awnings anchored to the structure.

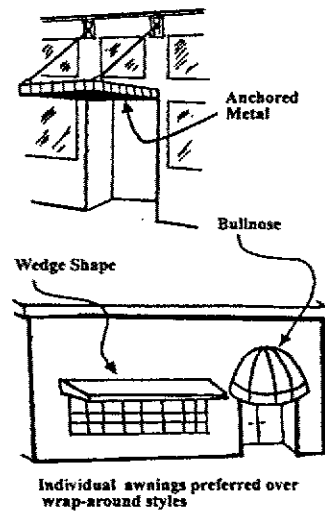


Figure 14. Preferred Awning Styles

- (f) For wedge style awnings, a narrow awning style is preferred.

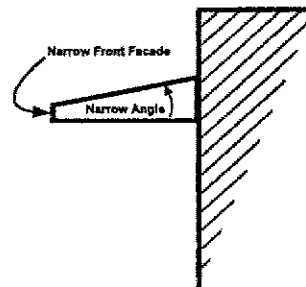


Figure 15. Preferred Style for Wedge shaped Awnings

- (g) Individual awnings over window and entrance awnings are preferred to wrap around awnings.
- (h) For awnings, it is preferable that the lettering and logo does not dominate the awning façade unless the lettering style and size is in keeping with the historic period of the structure.

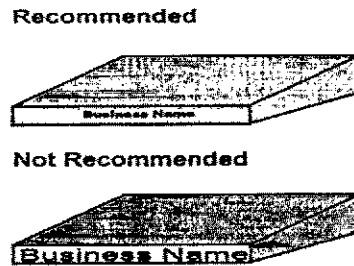


Figure 16. Name & Logo should not Dominate awning

- (i) The use of backlighting on fabric awnings is strongly discouraged.

20D.40.150-060 Pedestrian/Customer Elements.

- (1) Intent. The Old Town district should be emphasized as a pedestrian shopping and gathering environment, with comfortable and attractive sidewalks, plazas, informal seating areas, and pedestrian amenities that are consistent with the historic character of the district.
- (2) Design Criteria.
 - (a) Trees, Plants, and Flowers. The use of potted plants and flowers as well as street trees are encouraged, but should not impede pedestrian traffic.
 - (b) Street Lighting. Street lighting should relate in scale to the historic character of the area. The design of the light standards and luminaires should enhance an historic theme.
 - (c) Street Furniture. Public seating, trash receptacles, and informational/directional kiosks should be of uniform design and be provided throughout Old Town. Creation of informal gathering places that are integrated appropriately with the streetscape is encouraged.
 - (d) Sidewalks. Paving of sidewalks and other pedestrian rights-of-way should be constructed of a uniform material that is compatible with the historic pedestrian character. The private use of sidewalk rights-of-way areas may be appropriate for seasonal cafe seating or special displays.



FIGURE 1



FIGURE 2

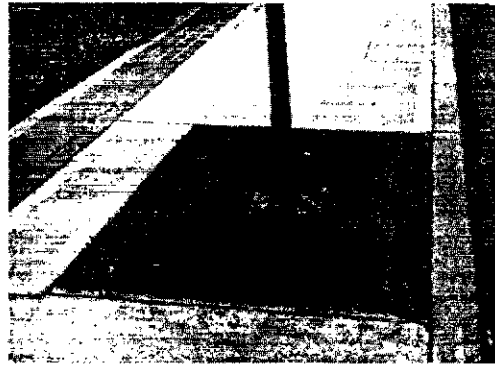


FIGURE 3

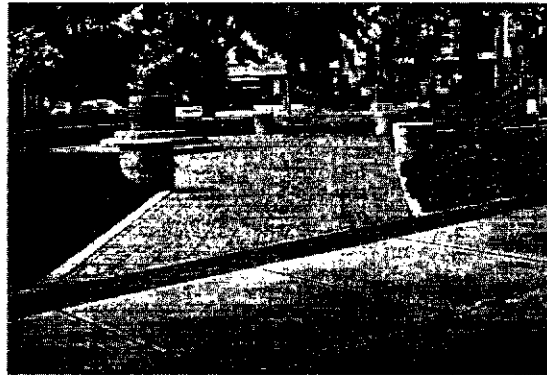


FIGURE 4



FIGURE 5

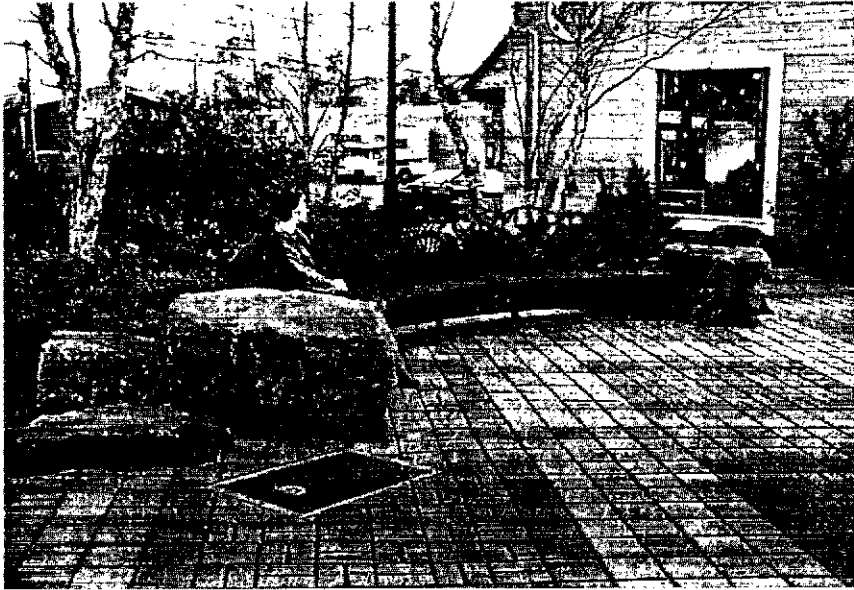


FIGURE 6

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